

# Welcome to the Wood Dale Open House for the Elgin O'Hare Western Access Project

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- ▶ This Open House is intended to provide you an opportunity to view up-to-date, preliminary details of the EOWA project
- ▶ Information on display includes:
  - ▶ Design plans
  - ▶ Illustrations of project features
  - ▶ Project timeline and construction schedule
  - ▶ Construction staging and maintenance of traffic information
  - ▶ Land acquisition process
- ▶ An interactive mapping tool provides information on travel through the area following completion of the EOWA project

**Please browse the exhibits and ask questions of project team members stationed throughout the room**



# Elgin O'Hare Western Access

## LEGEND

### Projects<sup>a</sup>

#### Elgin O'Hare

- US 20 to I-290 (2013-2015)
- I-290 to IL 83 (2014-2017)
- IL 83 to IL 19 (2017-2019)

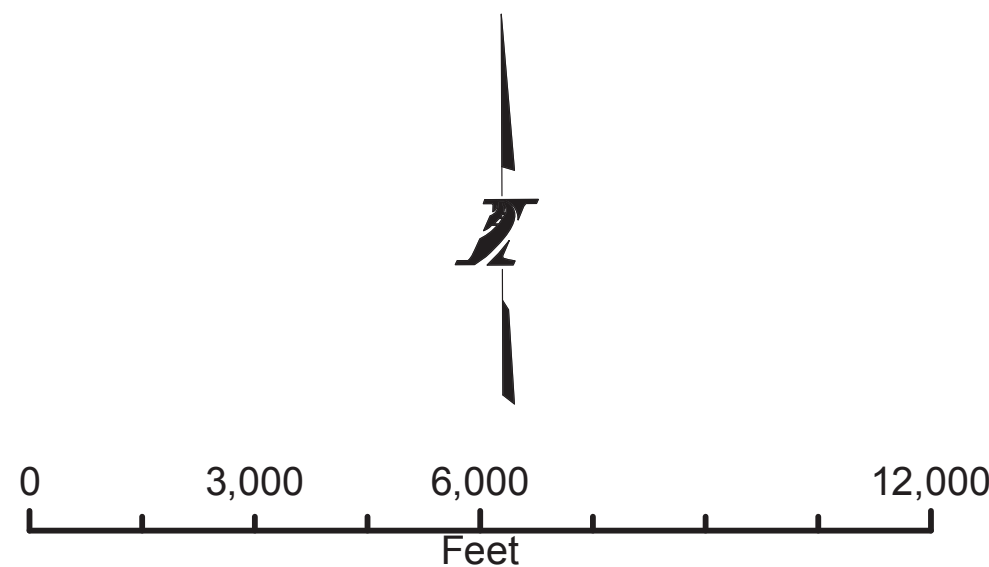
#### West Bypass

- I-294 at IL 64 (2020-2022)
- I-294 to IL 19 (2015-2022)
- Elgin O'Hare Corridor to I-90 (2021-2025)
- I-90 at Elmhurst Rd and Touhy Ave Corridor (2013-2019)

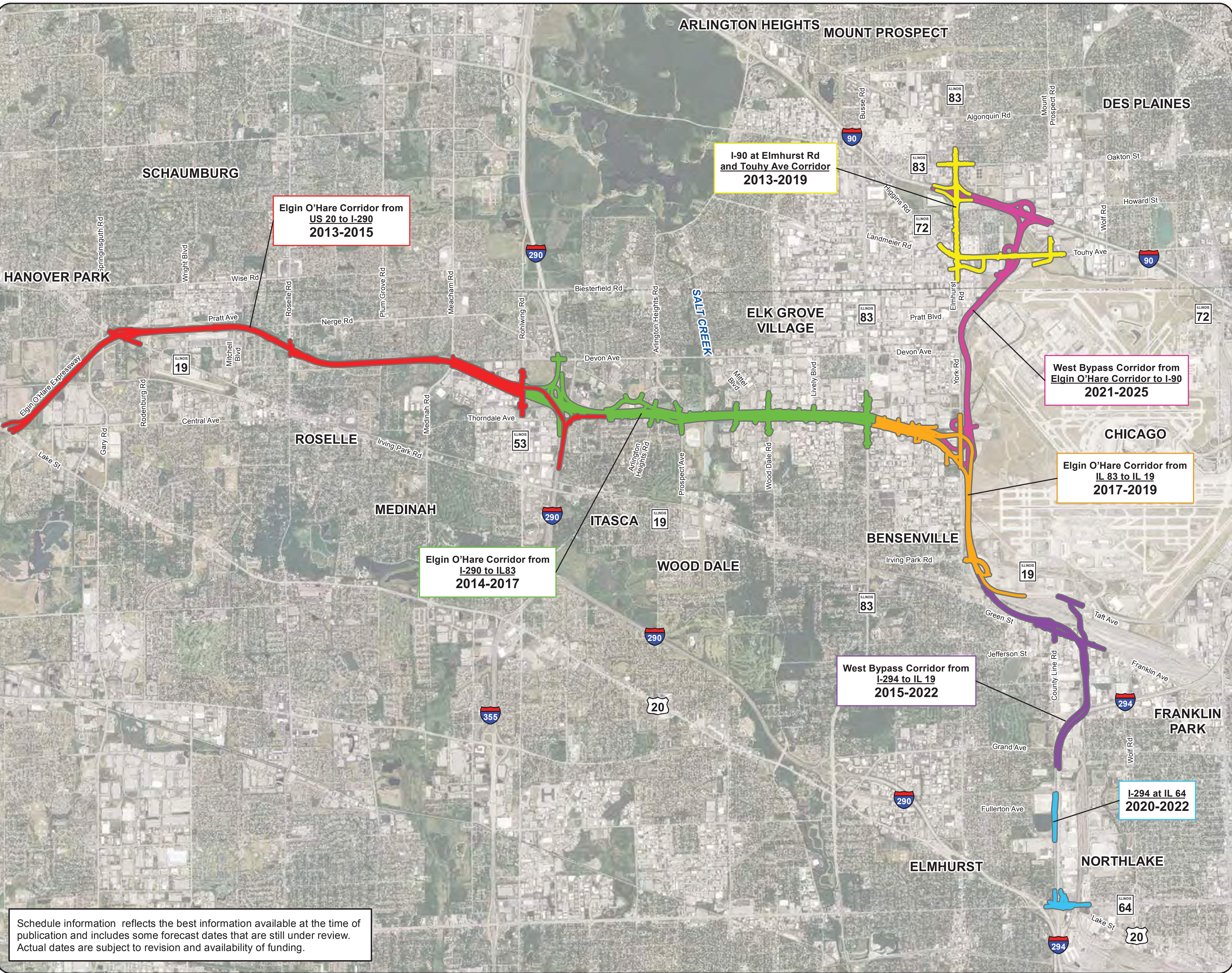
#### Corridor-Wide

- DCM/CCM
- Wetland Mitigation

<sup>a</sup> Projects include associated toll collection infrastructure



Construction  
Implementation Timeline  
February 19, 2014





Schedule information reflects the best information available at the time of publication and includes some forecast dates that are still under review. Actual dates are subject to revision and availability of funding.



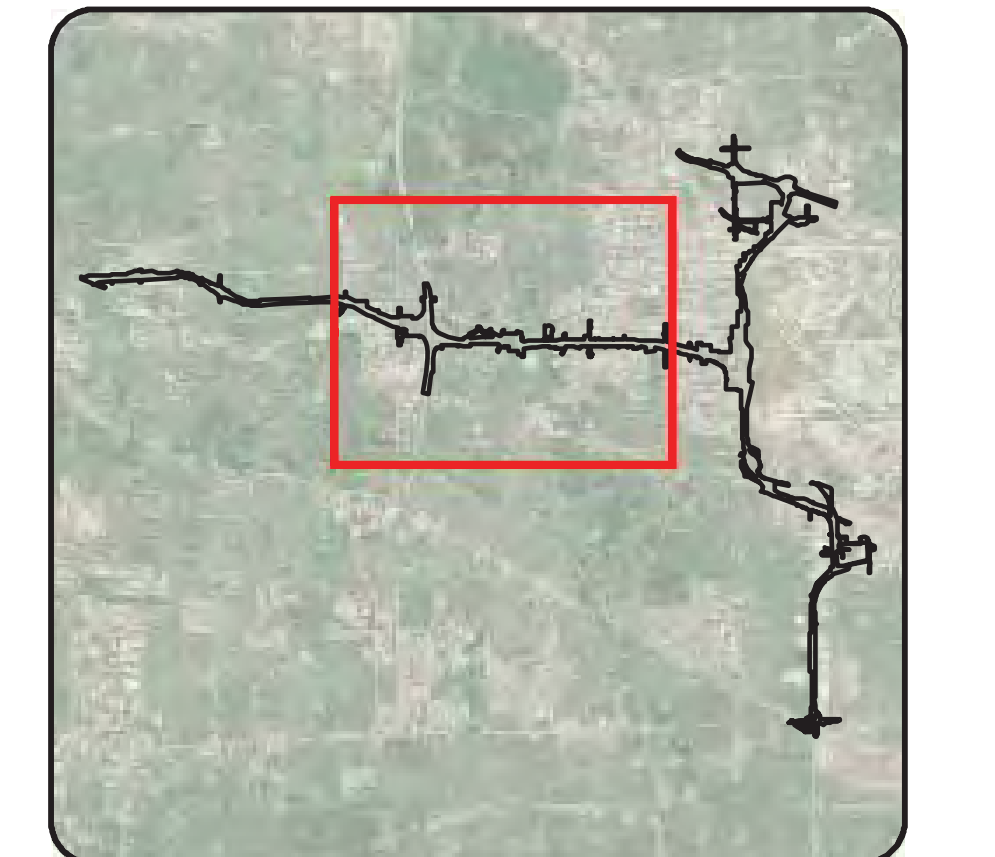
# Elgin O'Hare Western Access

## LEGEND

-  Bensenville and Wood Dale Boundaries
-  Future Construction

### 2013/2014 Scheduled Construction Contracts

- 1 Illinois Route 390 - Meacham Road/Medinah Road to Illinois Route 53
- 2 Illinois Route 53 (Rohwing Road) Bridge
- 3 Devon Avenue Bridge
- 4 I-290 Interchange
- 5 NB I-290 Flyover to WB Illinois Route 390
- 6 Hamilton Lakes Access Improvements
- 7 Illinois Route 390/Salt Creek/Mittel Boulevard Bridges
- 8 Wood Dale Road Bridge over Illinois Route 390 Mainline
- 9 Lively Boulevard Realignment and EB Frontage Road

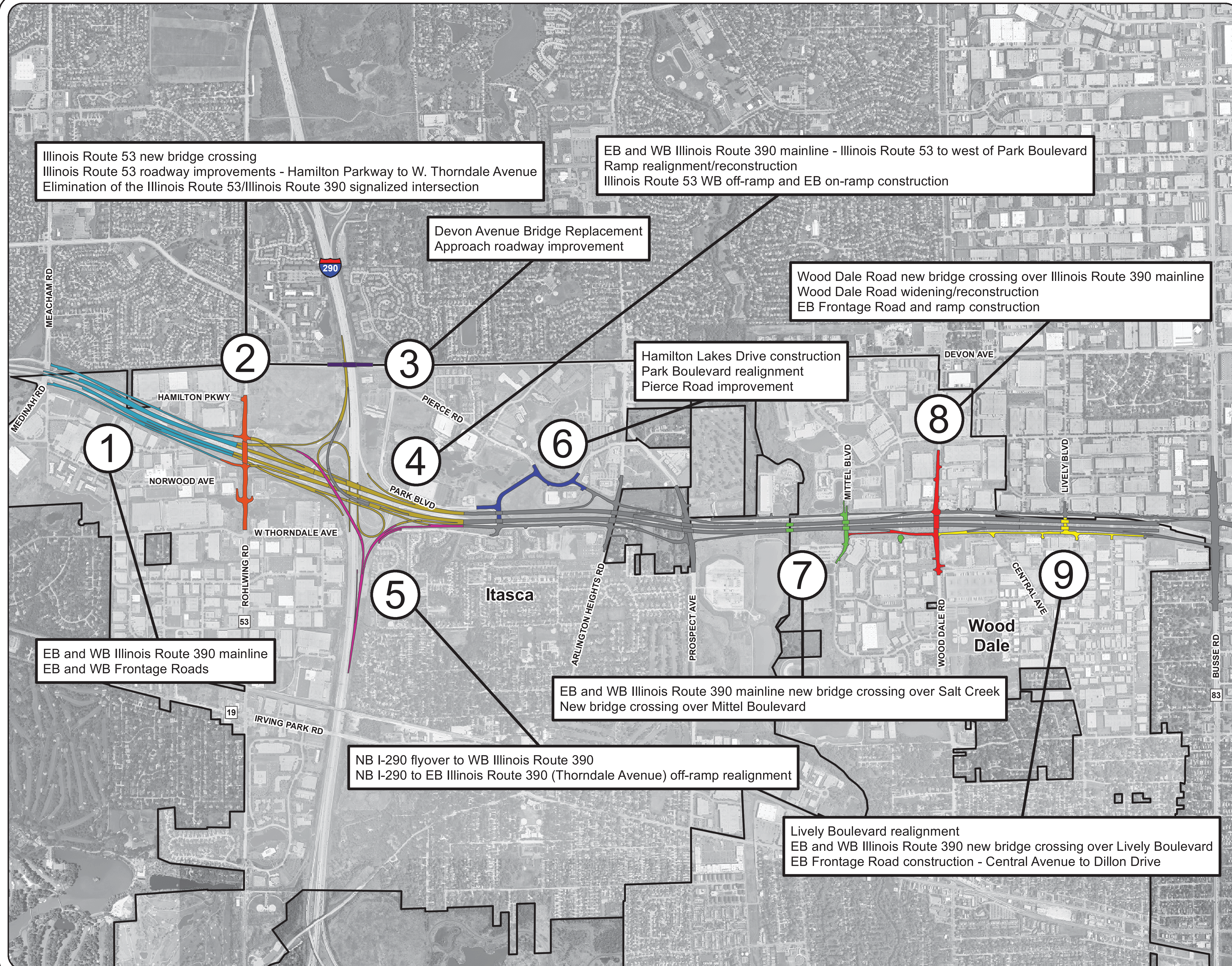


0 1,050 2,100 4,200  
Feet

## Exhibit 1

Elgin O'Hare Western Access  
Itasca and Wood Dale Areas  
Construction Phase Schedule

March 11 2014

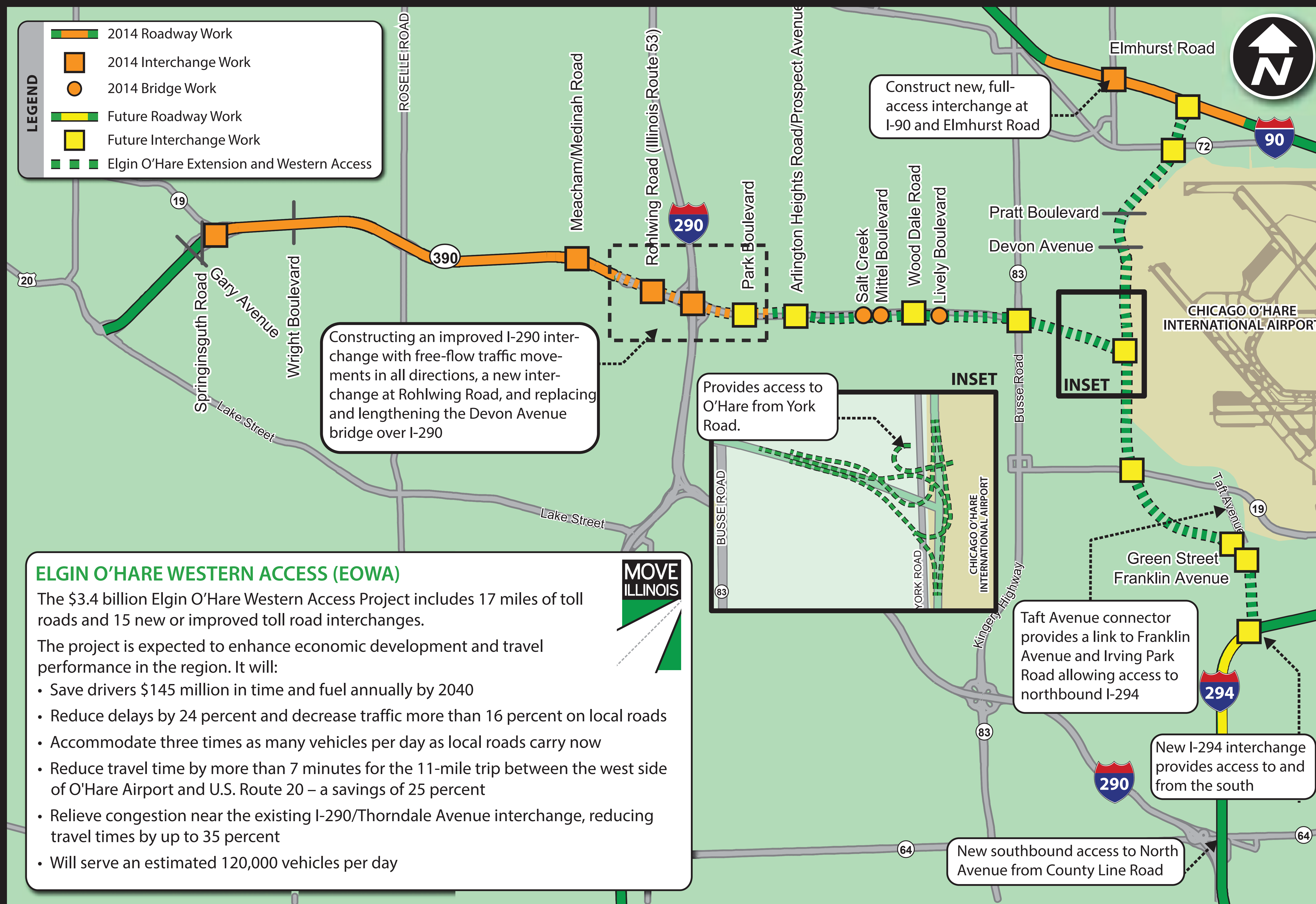






# ELGIN O'HARE WESTERN ACCESS

## A New, All-Electronic Toll Road



### ELGIN O'HARE WESTERN ACCESS (EOWA)

The \$3.4 billion Elgin O'Hare Western Access Project includes 17 miles of toll roads and 15 new or improved toll road interchanges.

The project is expected to enhance economic development and travel performance in the region. It will:

- Save drivers \$145 million in time and fuel annually by 2040
- Reduce delays by 24 percent and decrease traffic more than 16 percent on local roads
- Accommodate three times as many vehicles per day as local roads carry now
- Reduce travel time by more than 7 minutes for the 11-mile trip between the west side of O'Hare Airport and U.S. Route 20 – a savings of 25 percent
- Relieve congestion near the existing I-290/Thorndale Avenue interchange, reducing travel times by up to 35 percent
- Will serve an estimated 120,000 vehicles per day



# Elgin O'Hare Western Access

## Maintenance of Traffic Overview

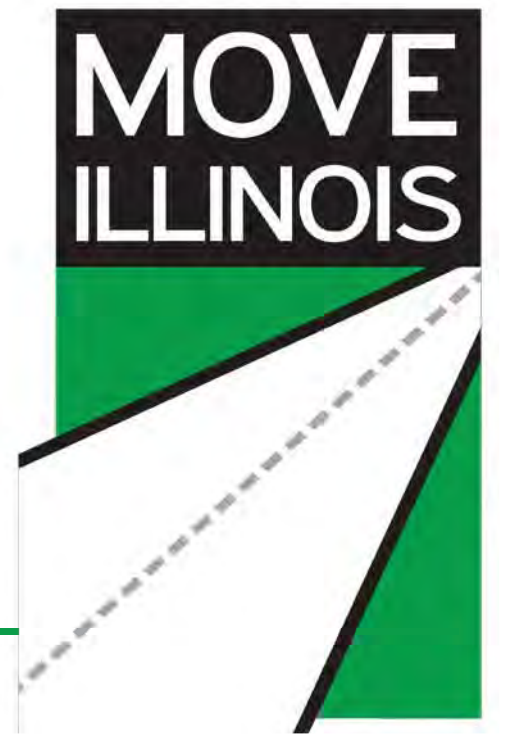
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- ▶ Whenever possible, the Tollway uses traffic shifts and shoulders to keep as many lanes open during peak hours as were available before construction
- ▶ Temporary off-peak closures will be required along Illinois Route 390, Thorndale Avenue and I-290
- ▶ Access to and from I-290 will be maintained during construction
- ▶ Local streets and arterials will remain open, but short-term lane closures may be required
- ▶ Construction will start on new segments first

# Elgin O'Hare Western Access Construction Management Office

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- ▶ **Construction office opening in March**

1555 Mittel Boulevard, Suite D

Wood Dale, Illinois

Phone: 630-422-1246

Open 8 a.m. to 4 p.m., Monday through Friday

- ▶ Pick up general information about the project
- ▶ Make an appointment to meet with engineering staff to ask questions and obtain more detailed information



# Elgin O'Hare Western Access

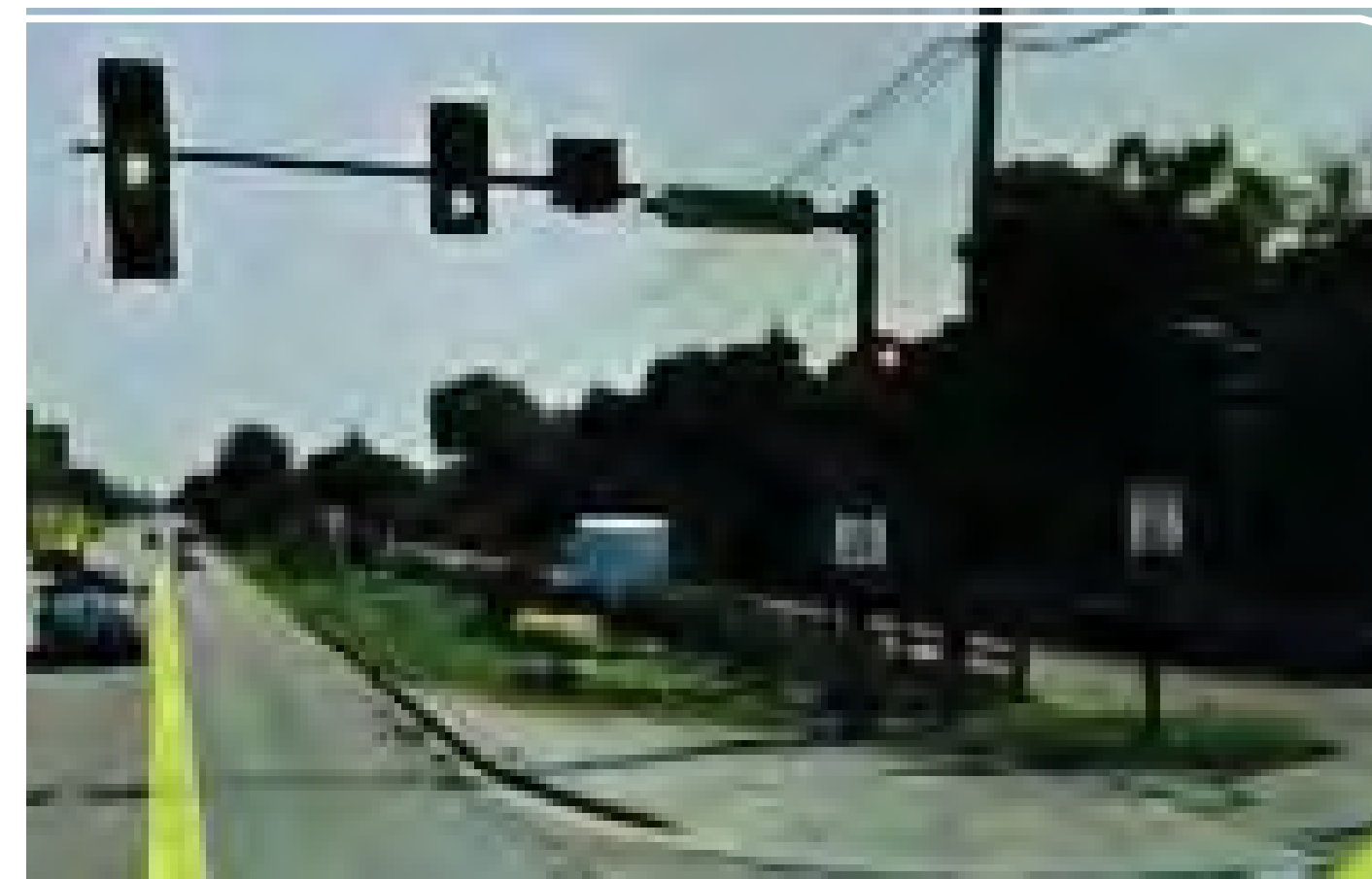
## Initial Tolling Concept – All-Electronic Mainline Tolling Plan

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- ▶ All-Electronic Tolling along entire length of project
  - ▶ First all-electronic roadway on Illinois Tollway system
  - ▶ No traditional tollbooths
  - ▶ Primarily uses I-PASS (more than 86 percent of present users)
  - ▶ Business rules and toll rate schedules under development
- ▶ Mainline tolling vs. traditional mainline and ramp tolling system
  - ▶ Tolls tied to miles traveled are more equitable than travel through a number of fixed points
  - ▶ Tolls will be assessed more frequently, but will generally be lower due to short distance between points
  - ▶ Encourages short- and long-distance trips (providing congestion relief on adjacent secondary roadways)

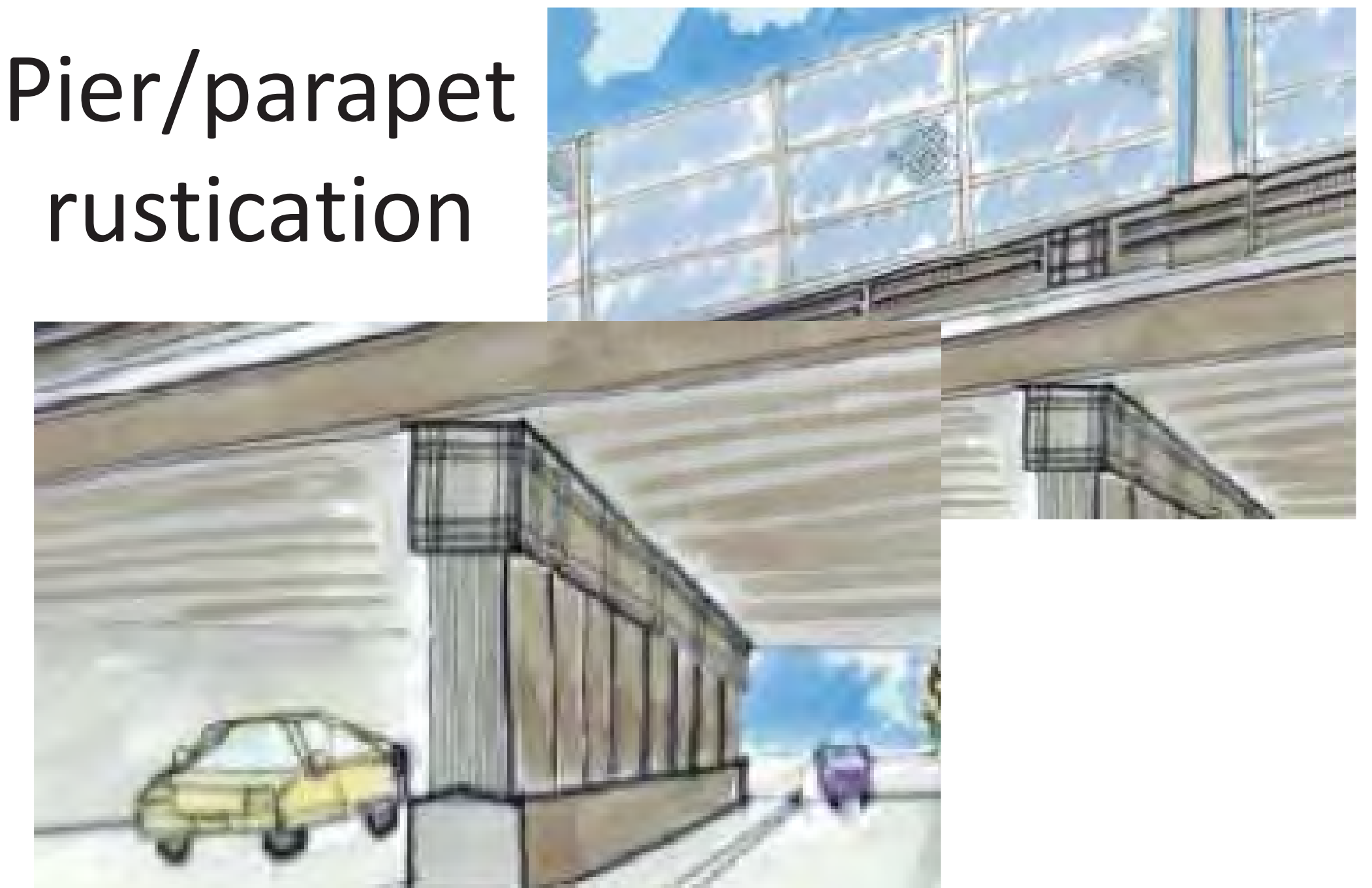
# Elgin O'Hare Western Access Tollway Aesthetic Commitments

Steel bridge paint

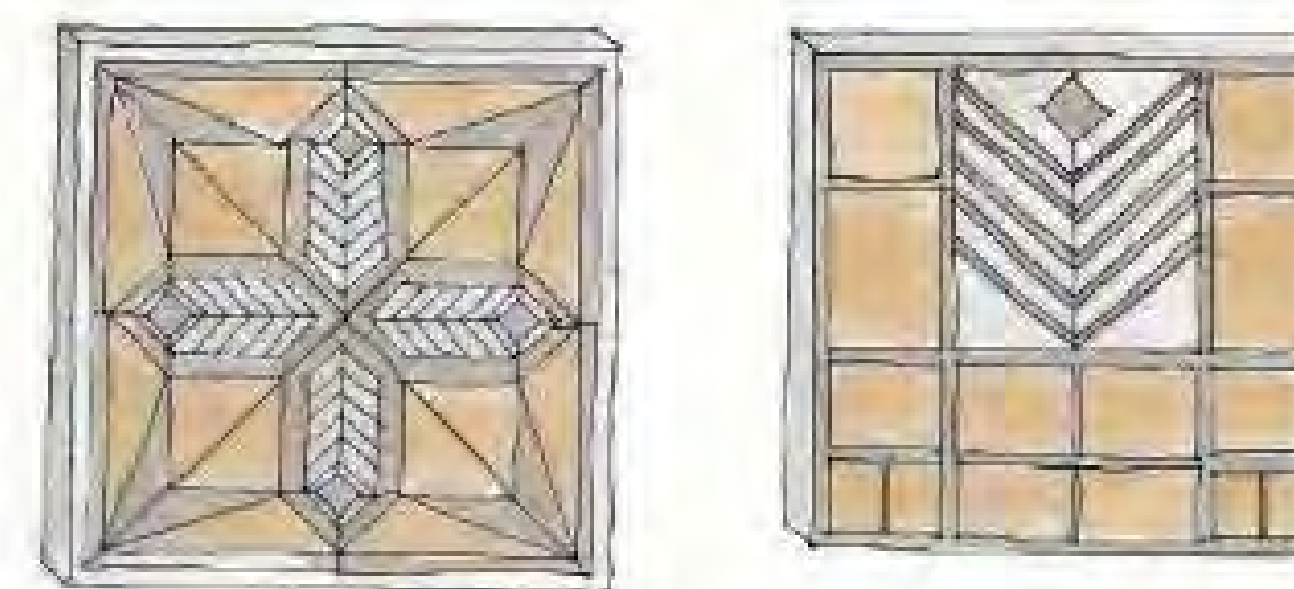


Black traffic signals

Pier/parapet  
rustication



Round piers for flyover bridges



Mechanically attached corridor  
identifier on local bridges  
(design to be determined)



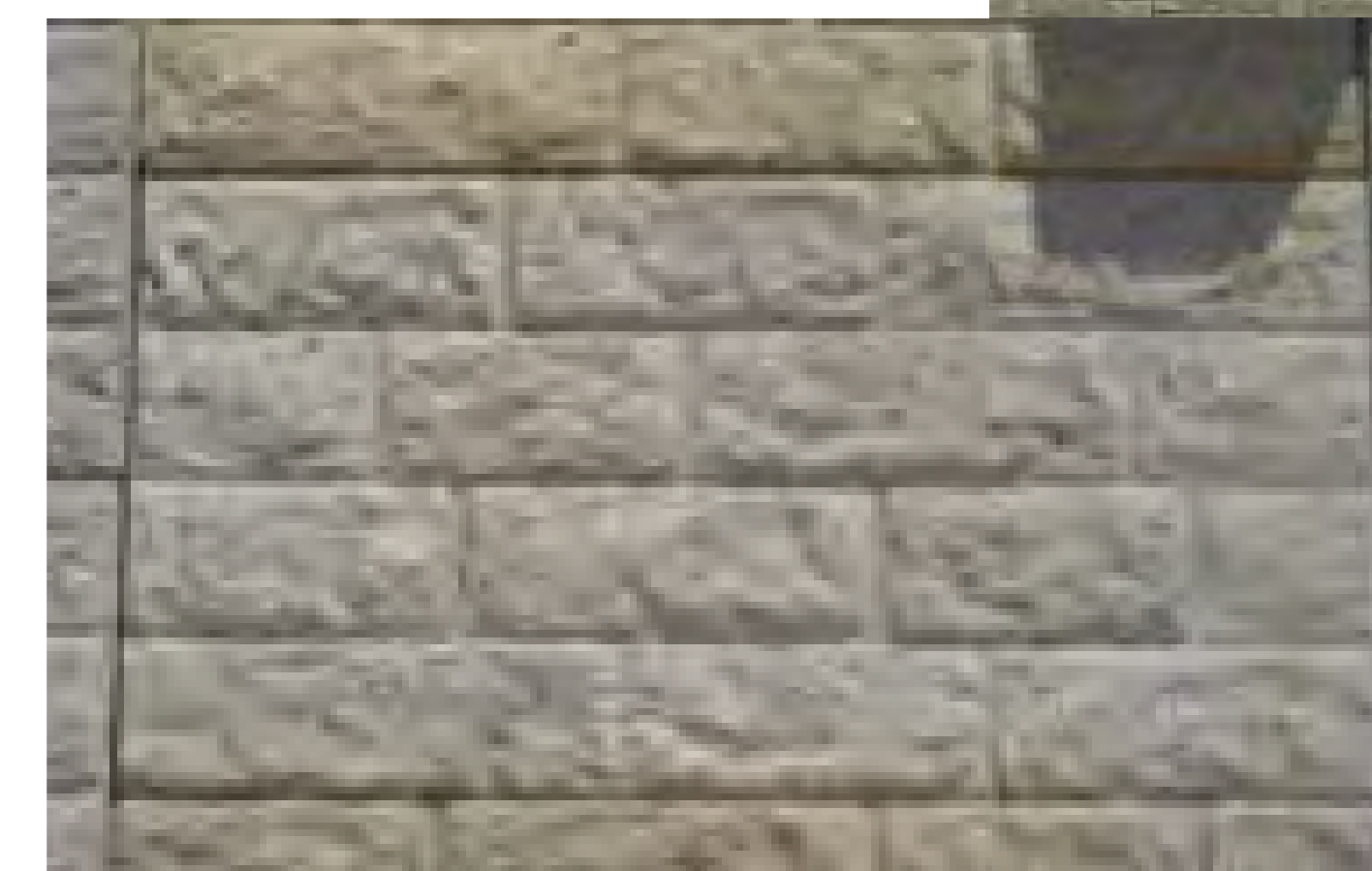
Ashlar stone  
noisewall  
pattern



Landscape



Leaf pattern on  
retaining walls



Limestone block  
retaining wall pattern

MOVE  
ILLINOIS

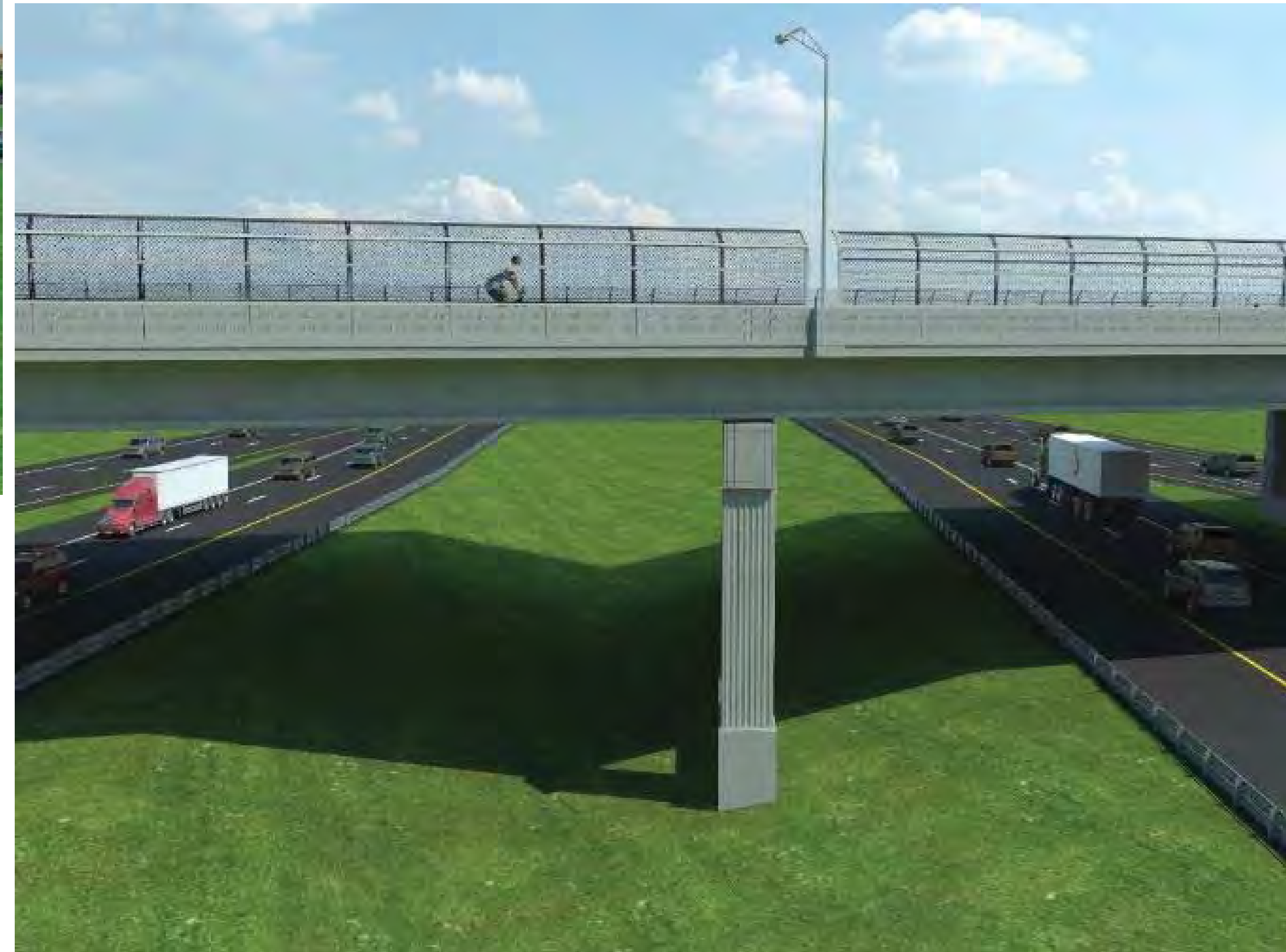


# Elgin O'Hare Western Access Example Tollway Applications

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Typical view of Illinois Tollway crossing over local road



Typical view of local road crossing over Illinois Tollway



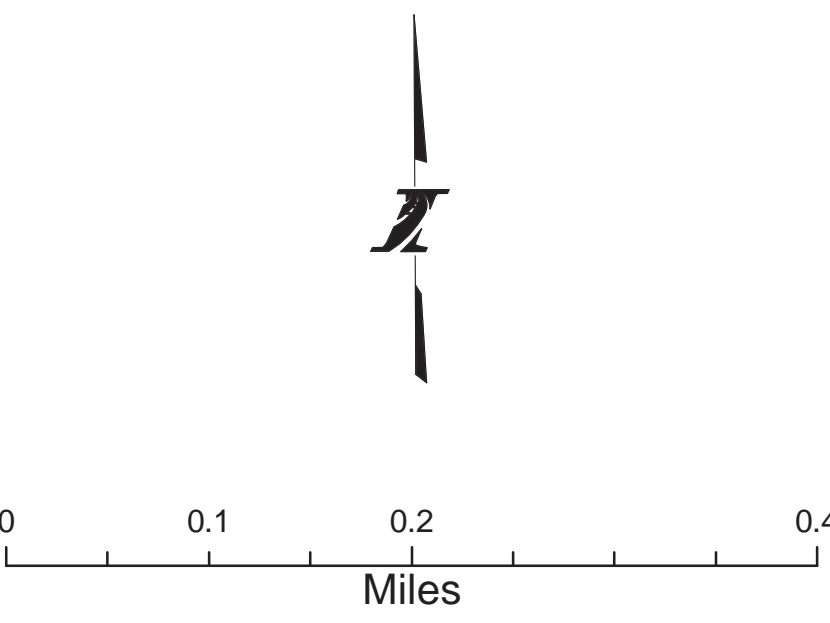
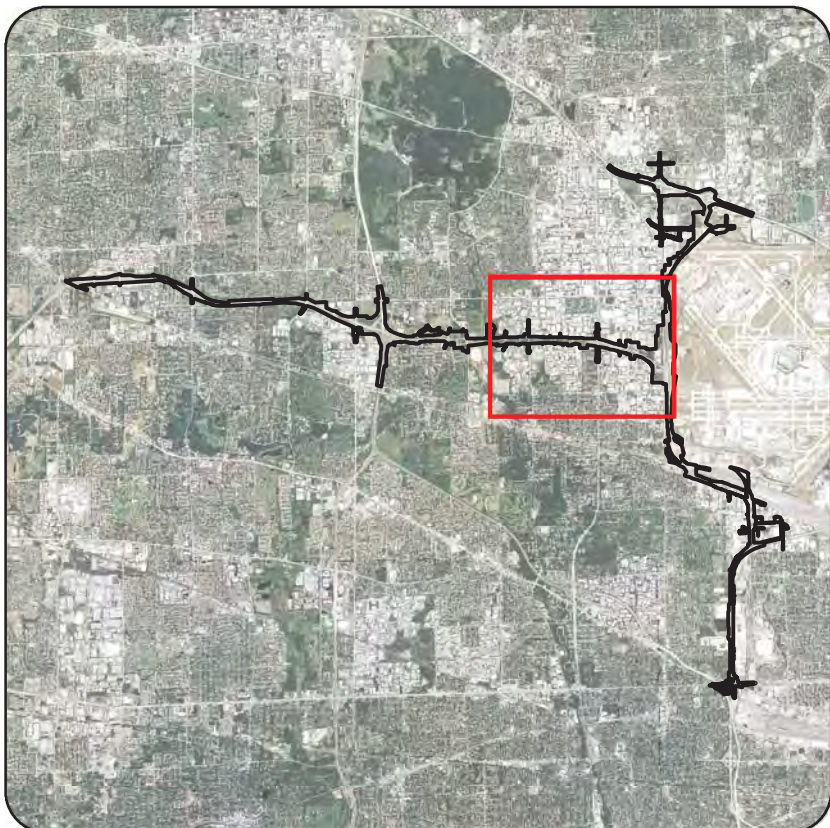


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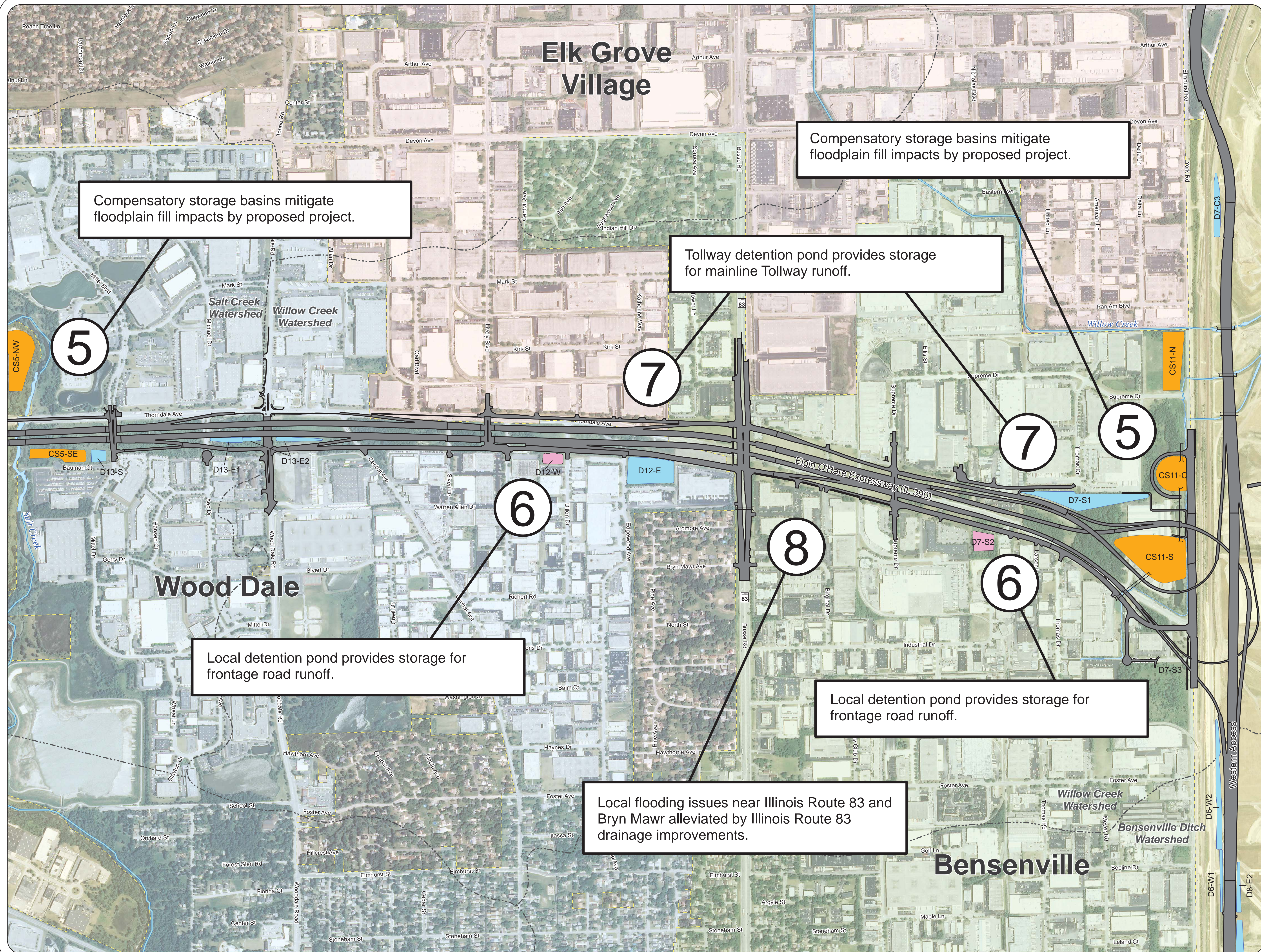
- Rivers
- Watershed Boundary
- Proposed EOWA Construction
- Proposed Compensatory Storage
- Proposed Detention (Local)
- Proposed Detention (Tollway)

Municipality

- Bensenville
- Chicago
- Elk Grove Village
- Wood Dale



EOWA Drainage:  
Wood Dale and  
Bensenville Areas



Compensatory storage basins mitigate floodplain fill impacts by proposed project.

Compensatory storage basins mitigate floodplain fill impacts by proposed project.

Tollway detention pond provides storage for mainline Tollway runoff.

Local detention pond provides storage for frontage road runoff.

Local flooding issues near Illinois Route 83 and Bryn Mawr alleviated by Illinois Route 83 drainage improvements.

Local detention pond provides storage for frontage road runoff.

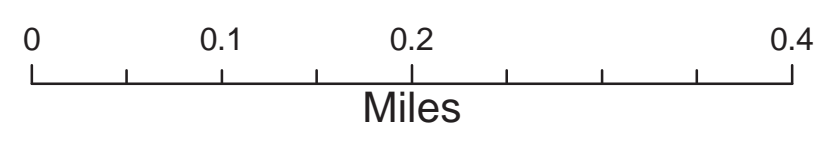
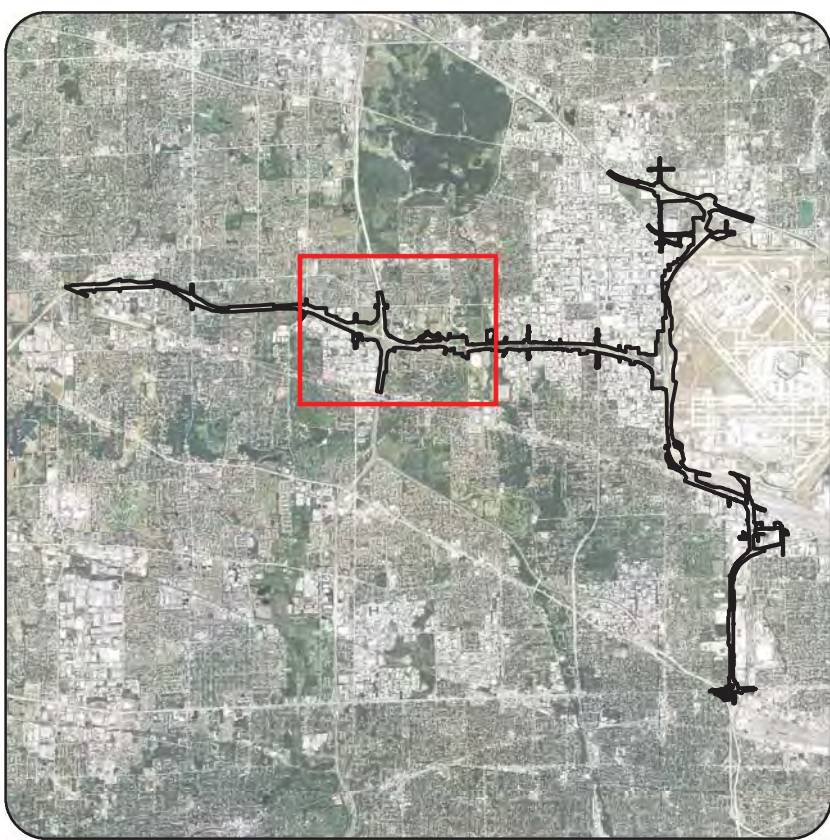


LEGEND

- Rivers
- Watershed Boundary
- Proposed EOWA Construction
- Proposed Compensatory Storage
- Proposed Detention (Local)
- Proposed Detention (IDOT)
- Proposed Detention (Tollway)

Municipality

- Elk Grove Village
- Itasca
- Wood Dale



EOWA Drainage:  
Itasca Area

Elk Grove  
Village

Wood  
Dale

Itasca

Medinah  
(unincorporated)

Northwest quadrant of I-290 Interchange:  
Significant detention volume provided  
within watershed to limit impacts downstream  
at Hamilton Lakes and Devon Avenue tributary  
to Salt Creek.

Thorndale Avenue culvert replacement: Existing  
culverts under Park Boulevard and Thorndale  
Avenue will be increased from 5 feet by 3 feet to  
9 feet by 4 feet. This will significantly reduce  
existing flooding upstream within residential area.

Tollway/ IDOT detention ponds provide  
storage for proposed roadway improvements.

Southeast quadrant of I-290 Interchange:  
Drainage routed east to 9 foot by 4 foot culvert  
instead of through Clover Ridge Subdivision.

